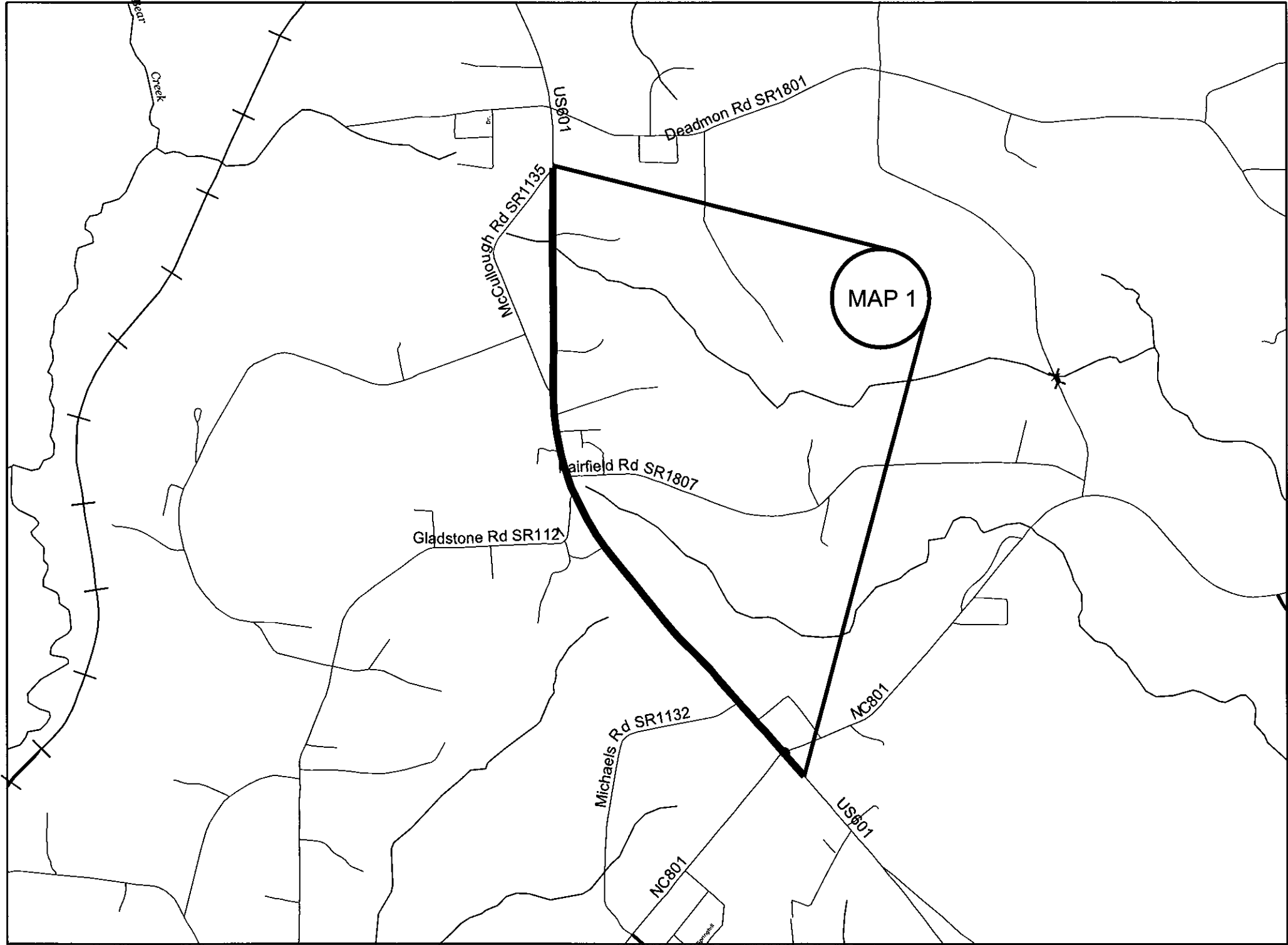


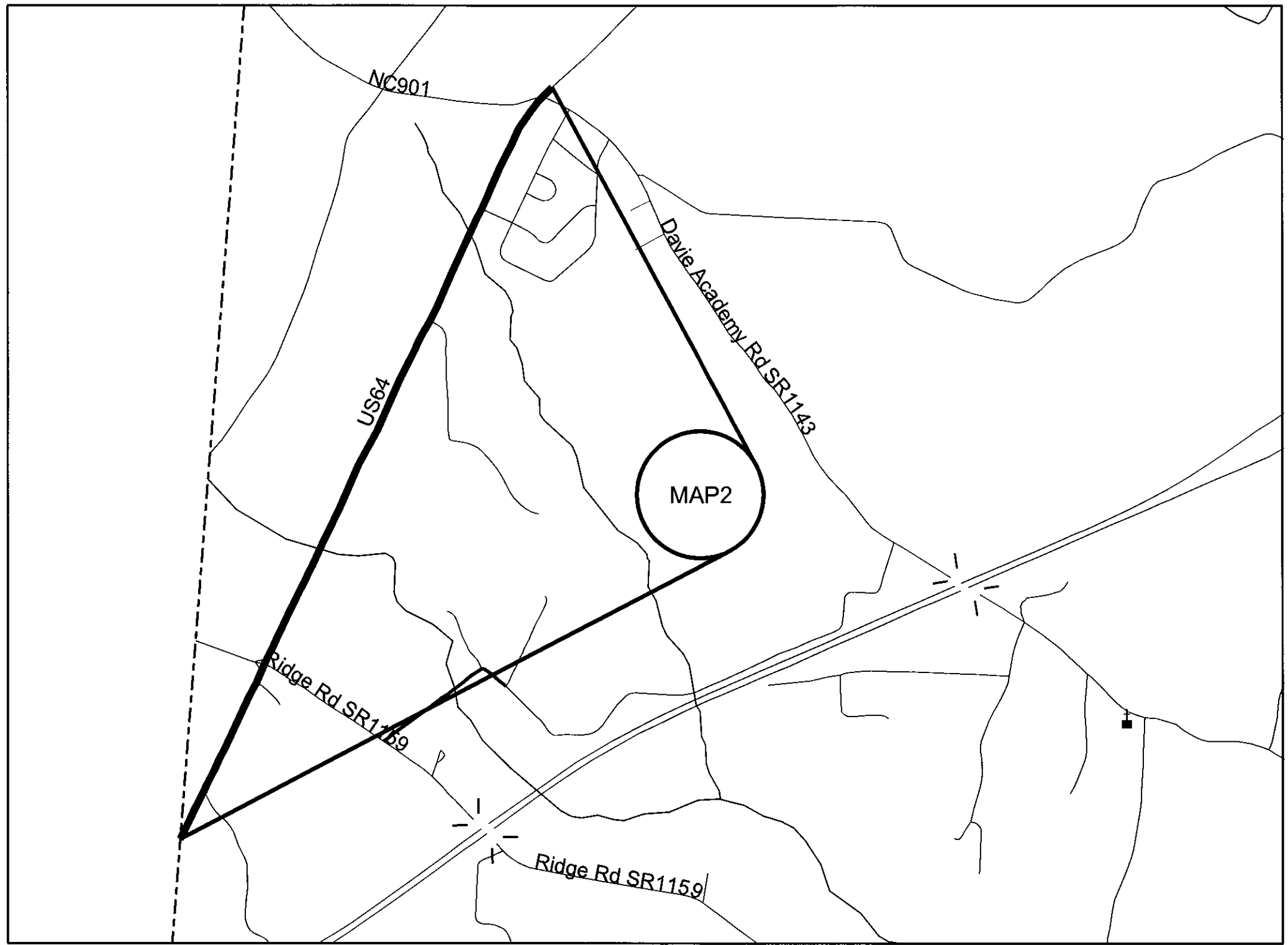
PROJECT REFERENCE NO.	SHEET NO.
2021CPT.09.03.10301 2021CPT.09.04.20301 2021CPT.09.04.20302	1



MAP 1 US601 from Approx. 100'
N. of McCullough Rd SR1135 to
end of lane taper S. of NC801
Mill 1 1/2" depth entire width
Pave 1 1/2" 9.5C

DAVIE COUNTY
NORTH CAROLINA

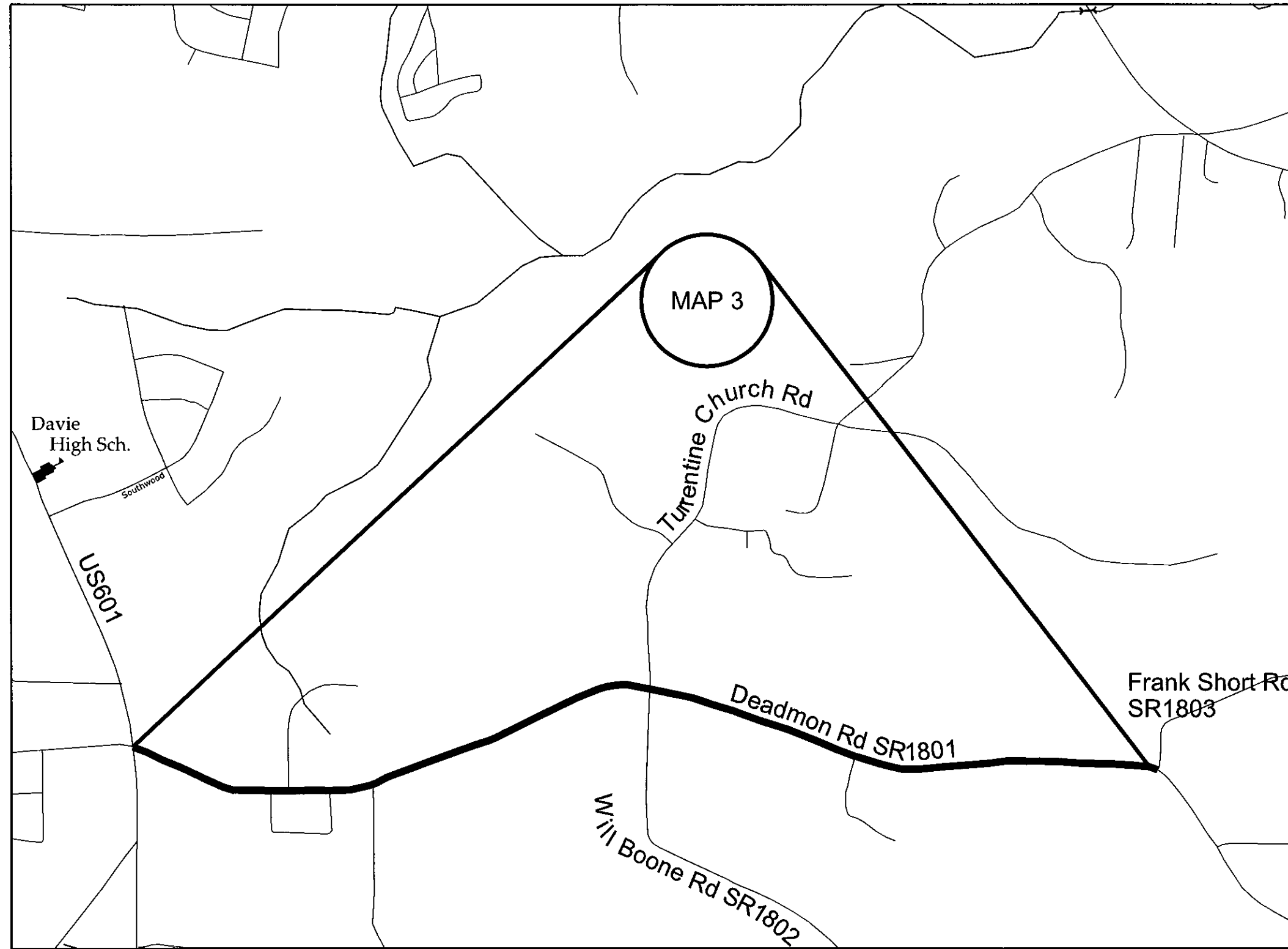
PROJECT REFERENCE NO.	SHEET NO.
2021CPT.09.03.10301 2021CPT.09.04.20301 2021CPT.09.04.20302	2



MAP 2 US64 from approx. 200' N
of NC901 to Iredell county line
Mill 1 1/2" depth entire width
Pave 1 1/2" 9.5C

DAVIE COUNTY
NORTH CAROLINA

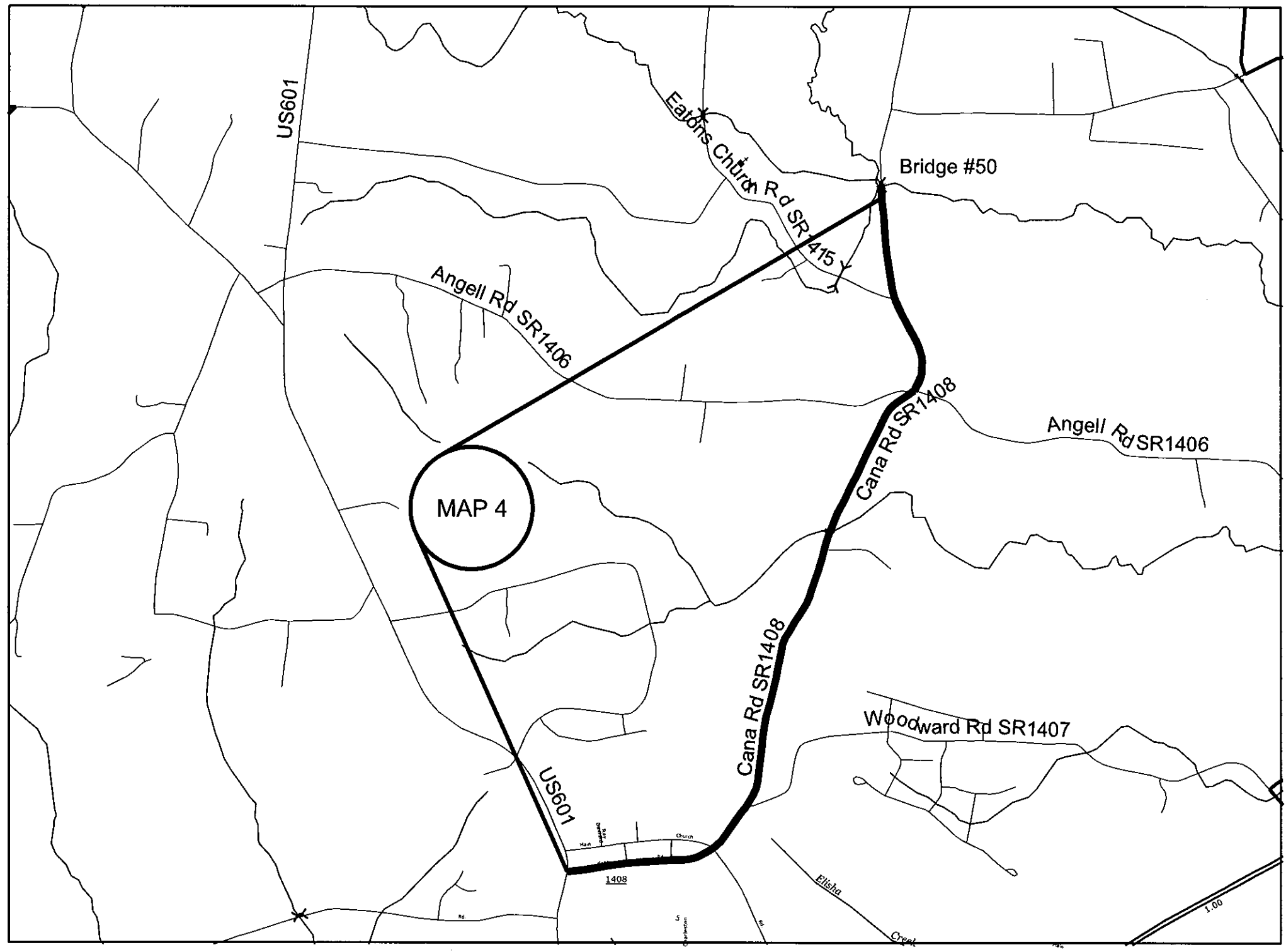
PROJECT REFERENCE NO.	SHEET NO.
2021CPT.09.03.10301 2021CPT.09.04.20301 2021CPT.09.04.20302	3



MAP 3 Deadmon Rd SR1801 from
US601 to Start of curve prior to
Frank Short Rd SR 1803
Mill 0- 1 1/2" Incidental mill at
begining, end and at all SR
intersections
Pave 1 1/2" 9.5B

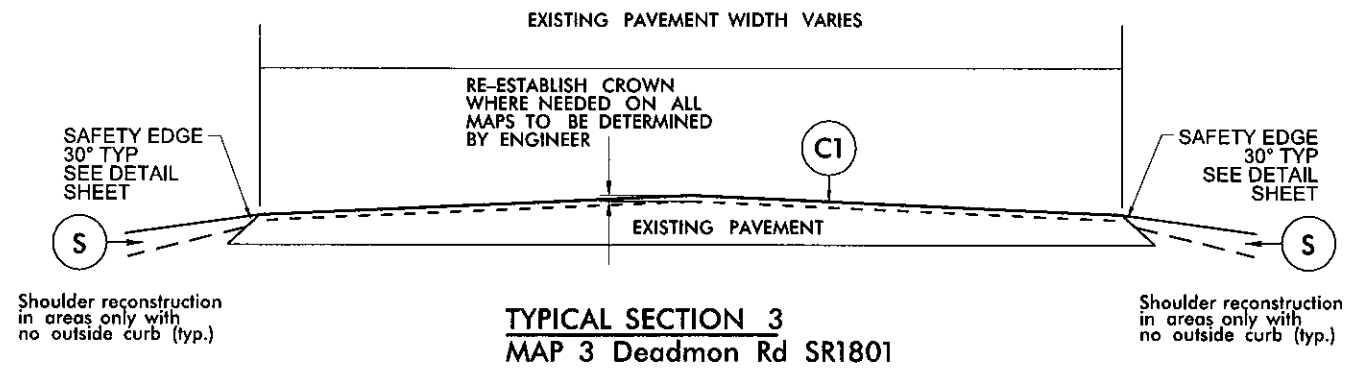
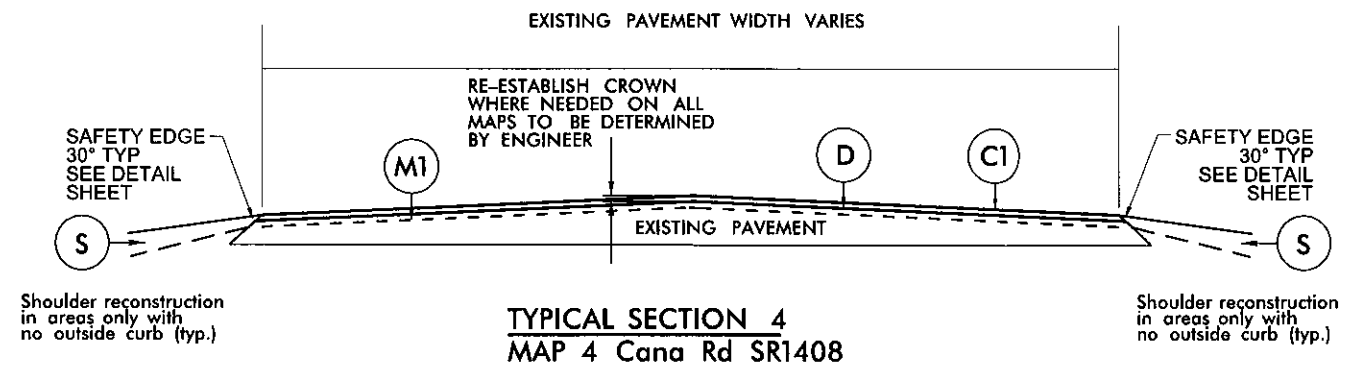
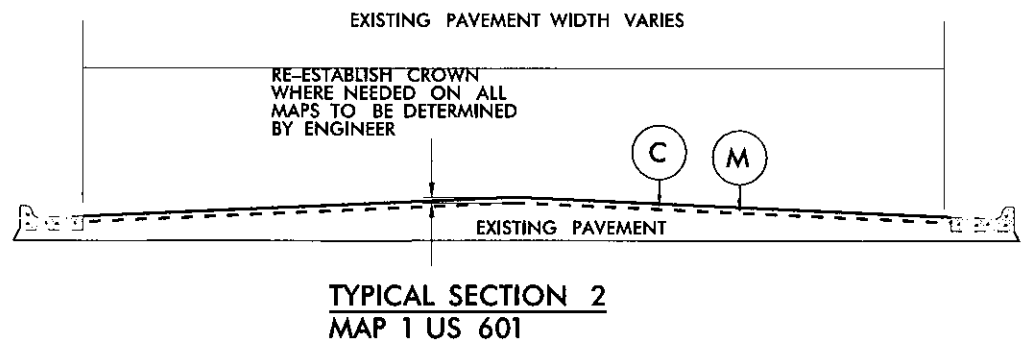
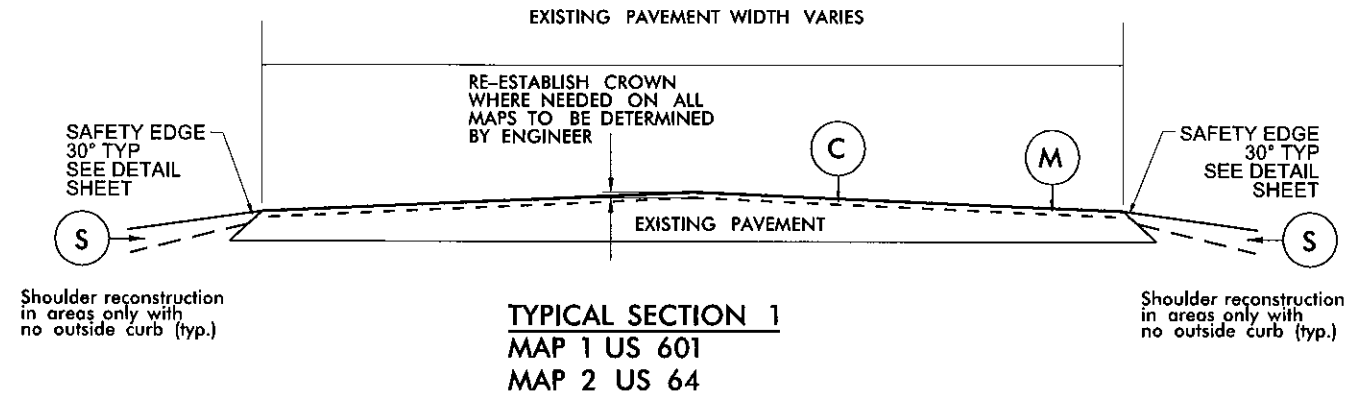
DAVIE COUNTY
NORTH CAROLINA

PROJECT REFERENCE NO.	SHEET NO.
2021CPT.09.03.10301 2021CPT.09.04.20301 2021CPT.09.04.20302	4

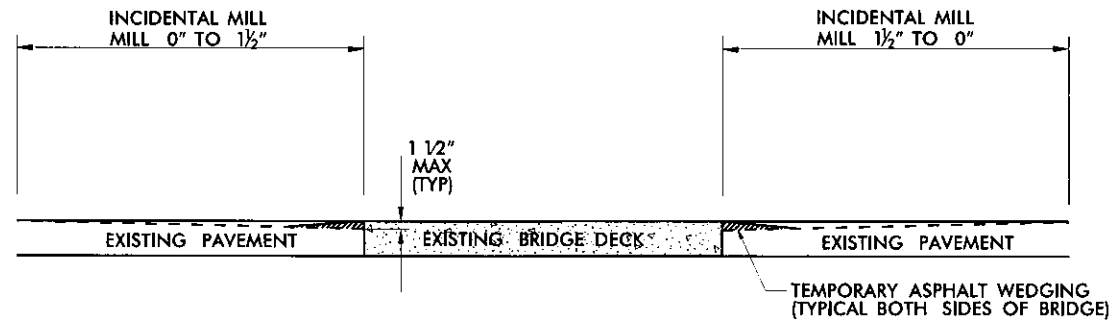


MAP 4 Cana Rd SR1408 from
 US601 to Pvt joint at Bridge #50
 Mill 1" depth entire width
 Mill 0- 2 1/2" incidental milling
 at begining, end and all SR
 intersections
 Pave 2" 19.0C
 Pave 1 1/2" 9.5B
 (Shoulder Reconstruction and
 Thermo-Striping to be done by
 State Forces)

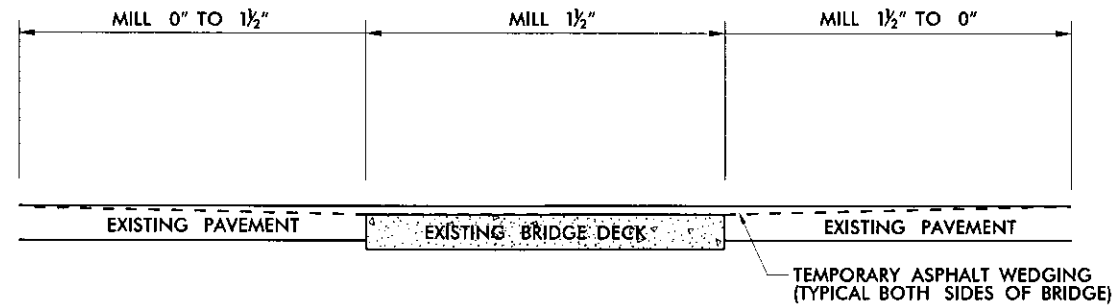
DAVIE COUNTY
 NORTH CAROLINA



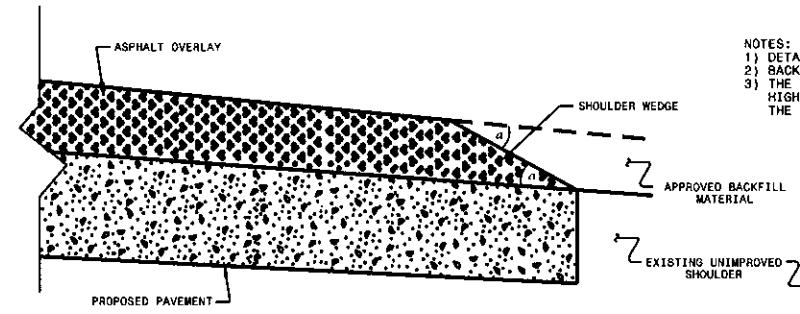
PAVEMENT SCHEDULE	
C	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, TO BE APPLIED AT AN AVERAGE RATE OF 168 LBS PER SQ. YD.
C1	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, TO BE APPLIED AT AN AVERAGE RATE OF 165 LBS PER SQ. YD.
D	PROP. APPROX. 2" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, TO BE APPLIED AT AN AVERAGE RATE OF 228 LBS PER SQ. YD.
M	MILL ASPHALT PAVEMENT, 1½" DEPTH
M1	MILL ASPHALT PAVEMENT, 1" DEPTH
S	SHOULDER RECONSTRUCTION (SEE DETAIL)



**INCIDENTAL MILLING
BRIDGE APPROACHES**
(SEE BRIDGE DATA SHEET)

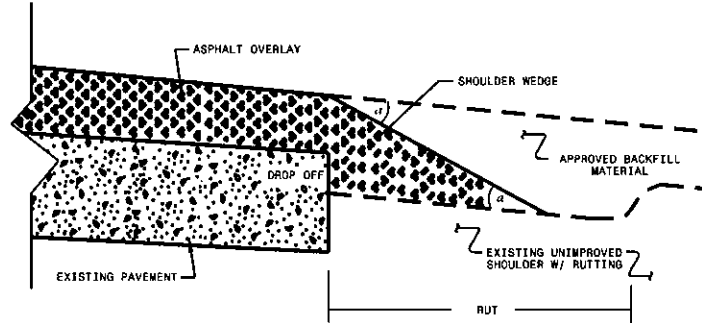


**INCIDENTAL MILLING
BRIDGE APPROACHES**
(SEE BRIDGE DATA SHEET)

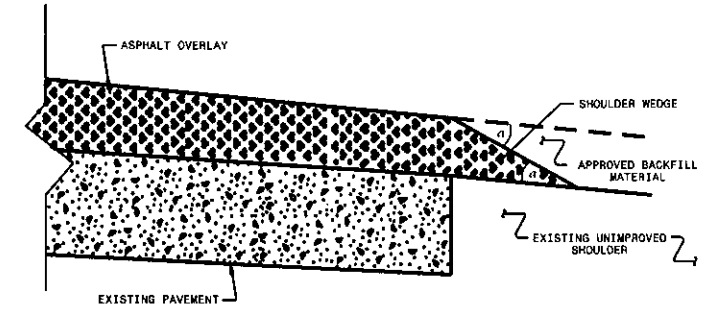


SHOULDER WEDGE DETAIL
(Resurfacing Projects w/ Widening or
with Existing Paved Shoulder having no dropoffs)

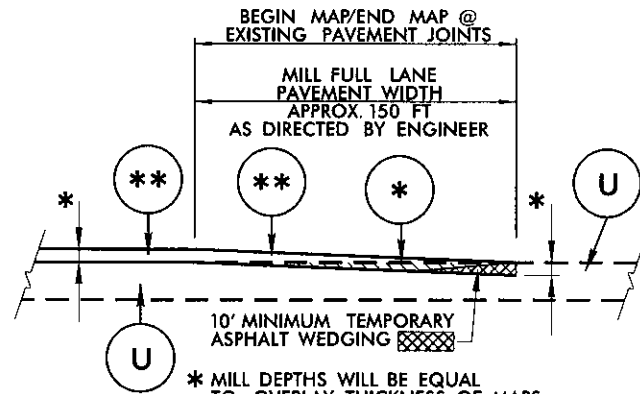
NOTES:
1) DETAIL DOES NOT APPLY TO OGAFD AND ULTRA-THIN BONDED WEARING COURSE.
2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS DIRECTED BY THE ENGINEER.



SHOULDER WEDGE DETAIL
(Resurfacing Adjacent to
Rutted Shoulder)

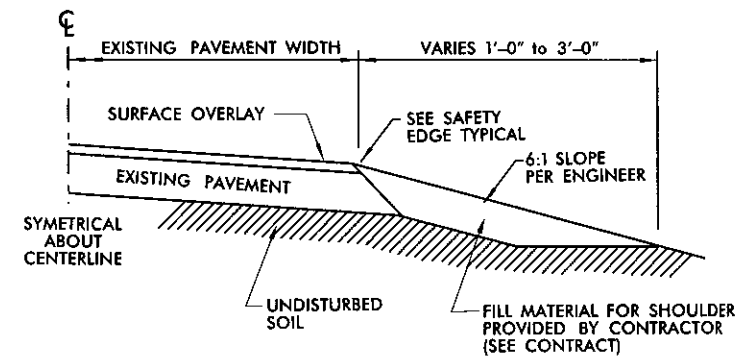


SHOULDER WEDGE DETAIL
(Resurfacing Projects w/ NO Widening)

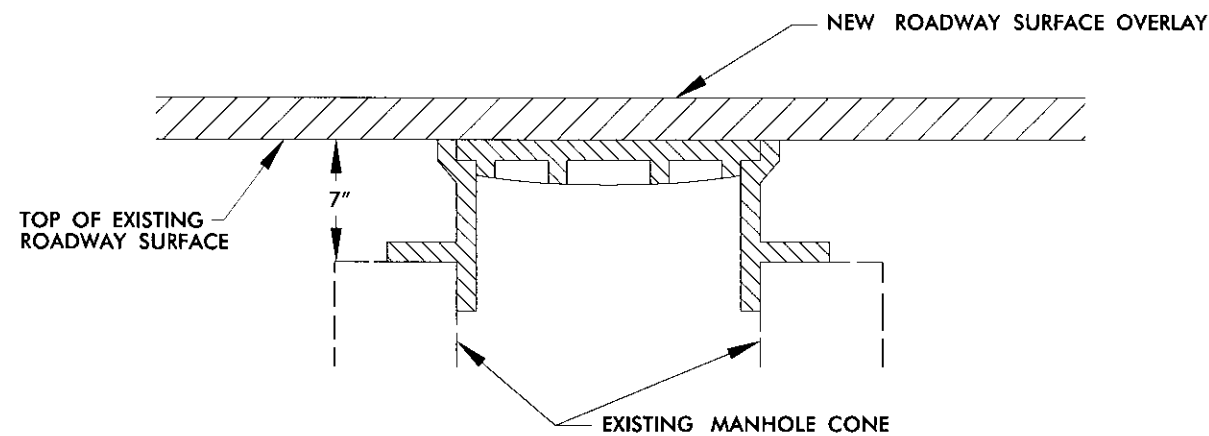


* MILL DEPTHS WILL BE EQUAL TO OVERLAY THICKNESS OF MAPS SEE TYPICALS AND BRIDGE DATA SHEETS
** MILL SR. Y-LINES APROX. 50' AS DIRECTED BY ENGINEER
*** SEE TYPICALS FOR MIX TYPE

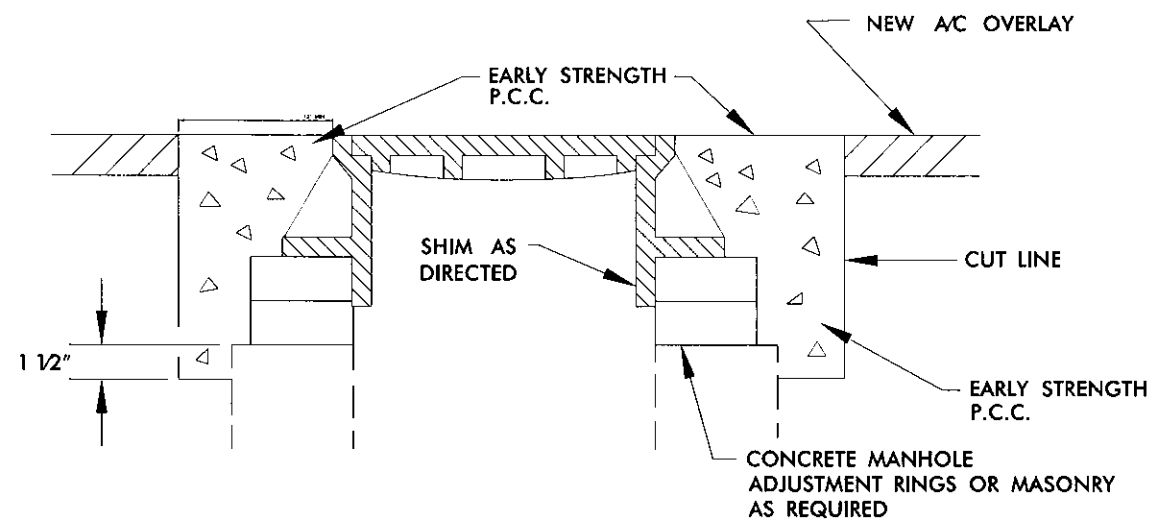
INCIDENTAL TIE-IN MILLING DETAIL



SHOULDER RECONSTRUCTION



STEP 1



STEPS 2,3, & 4

- STEP 1 COVER EXISTING MANHOLE WITH APPROVED MATERIAL AND CONSTRUCT OVERLAY ACROSS TOP OF MANHOLE
- STEP 2 SAW CUT EXCAVATION AROUND MANHOLE 12" MIN. FROM MANHOLE FRAME.
- STEP 3 RAISE MANHOLE FRAME RINGS TO FINISH PAVEMENT PROFILE AND CROSS SLOPE.
- STEP 4 BACKFILL WITH EARLY STRENGTH P.C.C. TO DEPTHS AS DIRECTED.

MANHOLE ADJUSTMENT DETAIL

CONSTRUCTION NOTES:

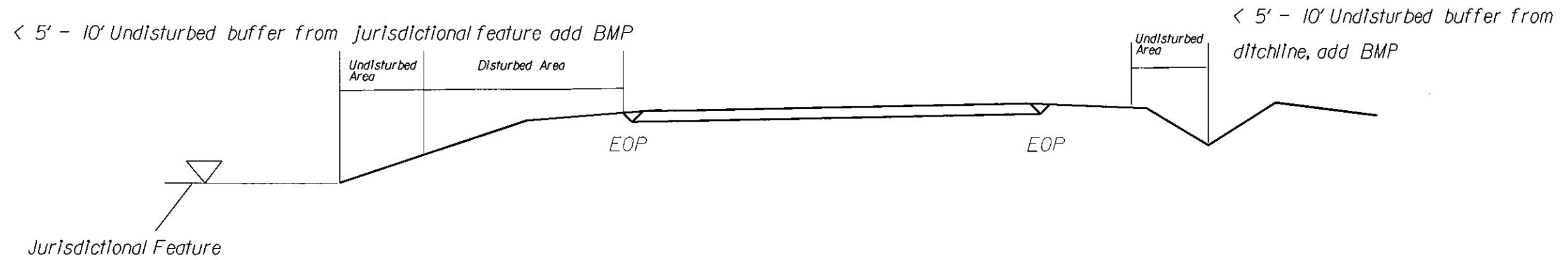
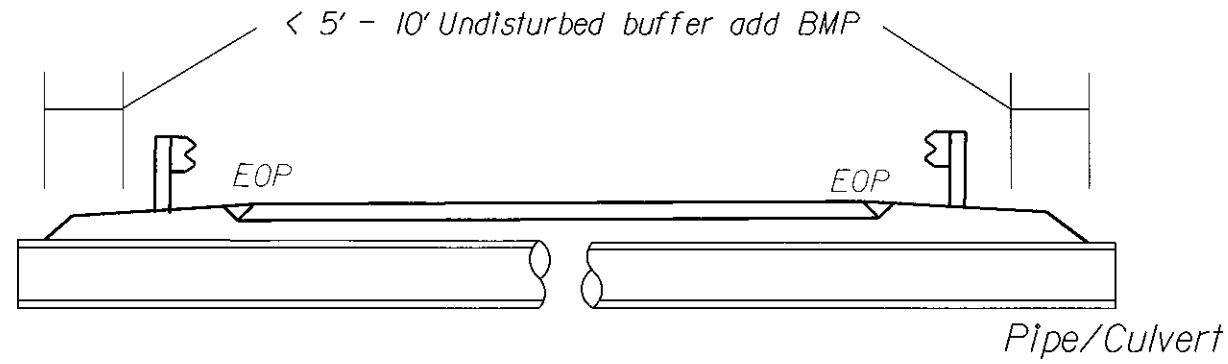
- ALL QUANTITIES ARE "ESTIMATED" AS INDICATED IN THE "SUMMARY OF QUANTITIES".
- CONSTRUCTION SHALL PROGRESS IN PHASES, IN THE ORDER INDICATED BELOW:
 - PHASE 1 - MILLING AND PATCHING (WHEN REQUIRED)
 - PHASE 2 - SURFACE OVERLAY
 - PHASE 3 - SHOULDER DROP-OFF REPAIR (AS NEEDED AND DIRECTED BY ENGINEER)
 - PHASE 4 - UTILITY ADJUSTMENTS (MANHOLE RING/COVER, VALVEMETER BOX RING/COVER, CATCH BASIN GRATE/COVER, DROP INLET GRATE/COVER, ETC.) WHEN REQUIRED.
- BRIDGES THAT HAVE FLOOR DRAINS, SHALL HAVE ALL FLOOR DRAINS LEFT OPEN. EXTRA CARE SHALL BE EXERCISED IN MILLING (IF REQUIRED) AND IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE.
- TEMPORARY ASPHALT WEDGING SHALL BE PLACED ON THE SAME DAY THAT BRIDGE AND/OR RAILROAD APPROACHES ARE MILLED (AND IF APPROACHES ARE MILLED PRIOR TO BRIDGE DECK).
- FOR TWO-LANE ROADWAYS - IT SHALL BE UNDERSTOOD THAT TYPICALLY ON A ROADWAY MEASURING 20 FEET OR LESS IN WIDTH, THE CENTER OF THE WHITE EDGELINE SHALL BE LOCATED SIX INCHES FROM THE EDGE OF PAVEMENT ON EITHER SIDE OF THE ROADWAY; ON A ROADWAY MEASURING 22 FEET IN WIDTH, TRAVEL LANES SHALL MEASURE 10 FEET AND THE WHITE EDGELINE SHALL BE LOCATED ONE FOOT FROM THE EDGE OF PAVEMENT ON EITHER SIDE; ON A ROADWAY MEASURING 24 FEET IN WIDTH, TRAVEL LANES SHALL MEASURE 11 FEET AND THE WHITE EDGELINE SHALL BE LOCATED ONE FOOT FROM THE EDGE OF PAVEMENT ON EITHER SIDE; ON A ROADWAY MEASURING 26 FEET OR MORE IN WIDTH, TRAVEL LANES SHALL MEASURE 12 FEET AND THE WHITE EDGELINE SHALL BE LOCATED NO LESS THAN ONE FOOT FROM THE EDGE OF PAVEMENT ON EITHER SIDE. THIS SHALL BE STANDARD PRACTICE UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- PAPER JOINTS ARE TO BE PLACED BETWEEN DAYS OF PAVING OPERATIONS AS SPECIFIED IN THE STANDARD SPECIFICATIONS SECTION 610-11.
- ALL MILLED AREAS WILL BE PAVED WITHIN 72 HOURS UNLESS APPROVED BY THE ENGINEER.
- REPLACE ANY PORTION OF STOP BARS AND OTHER PAVEMENT MARKINGS AT ANY INTERSECTION INCLUDING Y-LINES NOT ACTUALLY BEING PAVED OVER, THAT ARE OBLITERATED BY THE PAVING OPERATION EITHER BY HAULING WHEEL TRACKS OR TACK TRUCK BY THE END OF EACH RESURFACING OPERATION

NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

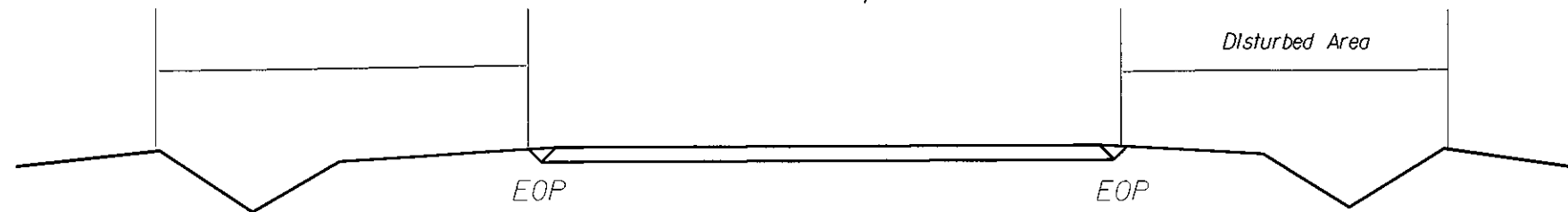
BMP Options: Wattle or Silt Fence

EROSION CONTROL DETAIL

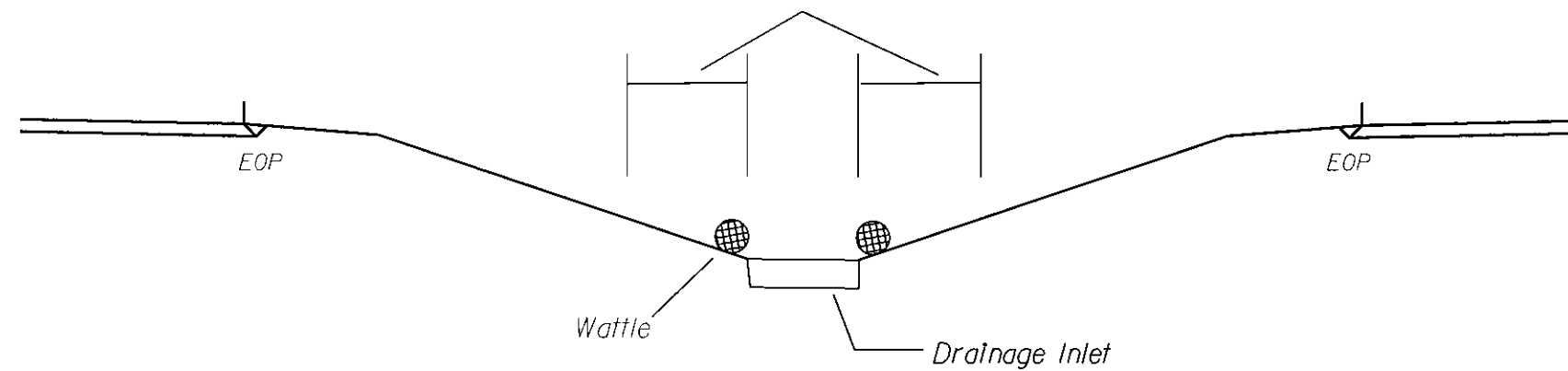
PROJECT REFERENCE NO.	SHEET NO.
2021CPT.09.03.10301 2021CPT.09.04.20301 2021CPT.09.04.20302	8



Use BMP's if shoulders and/or front slopes and/or ditchline and/or backslopes are disturbed



< 5' - 10' Undisturbed buffer from inlet, add wattle



NOT TO SCALE

PROJECT NO.	SHEET NO.	TOTAL NO.
1CPT.09.03.10301, 2021CPT.09.04.20	10	
2021CPT.09.04.20302,	10	

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	0106000000-E	1220000000-E	1245000000-E	1297000000-E	1330000000-E	1503000000-E	1519000000-E	1523000000-E	1575000000-E	1704000000-E	2830000000-N	2845000000-N	6000000000-E	6071010000-E				
												BORROW EXCAVATION	INCIDENTAL STONE BASE	SHOULDER RECONSTRUCTION	MILLING ASPHALT PAVEMENT, 1 1/2" DEPTH	MILLING ASPHALT PAVEMENT, 1" DEPTH	INCIDENTAL MILLING	INTERMEDIATE COURSE, 119.0C	SURFACE COURSE, S9.5B	SURFACE COURSE, S9.5C	ASPHALT BINDER FOR PLANT MIX	PATCHING EXISTING PAVEMENT	ADJ. OF MANHOLES	ADJ. OF METER OR VALVE BOX	TEMPORARY SILT FENCE	WATTLE			
												MI	FT	CY	TONS	SMI	SY	SY	SY	TONS	TONS	TONS	TONS	TONS	EA	EA	LF	LF	
2021CPT.09.03.10301	Davie	1	US601	FROM APPROX. 100' N. OF MCCULLOUGH RD SR1135 TO END OF LANE TAPER S. OF NC801	1, 2	2	2WU	NO	NO	2.15	27	258	310	5.07	43,819				3,920	235	15	1	3	860	86				
TOTAL FOR MAP NO. 1												2.15		258	310	5.07	43,819				3,920	235	15	1	3	860	86		
2021CPT.09.03.10301	Davie	2	US64	FROM APPROX. 200' N OF NC901 TO IREDELL COUNTY LINE	1	2	2WU	NO	NO	2.202	26	264	120	4.40	35,143				3,262	196	10			881	88				
TOTAL FOR MAP NO. 2												2.202		264	120	4.40	35,143				3,262	196	10			881	88		
TOTAL FOR PROJ NO. 2021CPT.09.03.10301												4.352		522	430	9.47	78,962				7,182	431	25	1	3	1,741	174		
												78,962																	
2021CPT.09.04.20301	Davie	3	DEADMON RD SR 1801	FROM US601 TO START OF CURVE PRIOR TO FRANK SHORT RD SR1803	3	2	2WU	NO	NO	2.254	22	270	156	4.51				1,200	2,697		181	10			902	90			
TOTAL FOR MAP NO. 3												2.254		270	156	4.51				1,200	2,697		181	10			902	90	
2021CPT.09.04.20301	Davie	4	CANA RD SR1408	FROM US601 TO PVT JOINT AT BRIDGE # 50	4	2	2WU	NO	NO	3.358	22		230					1,444	4,020		269				1,343	134			
TOTAL FOR MAP NO. 4												3.358			230					1,444	4,020		269					1,343	134
TOTAL FOR PROJ NO. 2021CPT.09.04.20301												5.612		270	386	4.51				2,644	6,717		450	10				2,245	224
2021CPT.09.04.20302	Davie	4	CANA RD SR 1408	FROM US601 TO PVT JOINT AT BRIDGE #50	4	2	2WU	NO	NO	3.358	22							44,052			267								
TOTAL FOR MAP NO. 4												3.358								44,052			267						
TOTAL FOR PROJ NO. 2021CPT.09.04.20302												3.358								44,052			267						
												44,052																	
GRAND TOTAL												13.322		792	816	13.98	78,962	44,052	2,644	5,565	6,717	7,182	1,148	35	1	3	3,986	398	
												123,014																	

Note: All quantities listed include turn lanes and are estimates; Payment will be based on actual field measurements and quantities received

PROJECT NO.	SHEET NO.	TOTAL NO.
PT.09.03.10301, 2021CPT.09.04.20302	11	
	11	

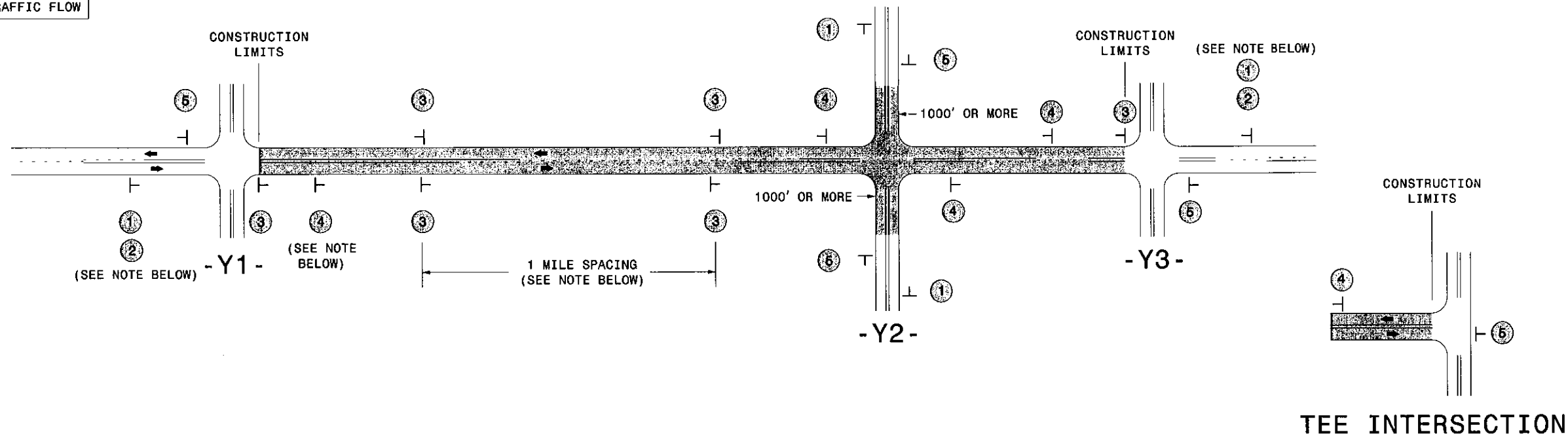
THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	4413000000-E	4457000000-N	4685000000-E		4695000000-E	4710000000-E	4725000000-E			4810000000-E		4820000000-E	4835000000-E	4905000000-N			
										WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	4" X 90 M WHITE THERMO	4" X 90 M YELLOW THERMO	8" X 90 M YELLOW THERMO	24" X 120 M WHITE THERMO	THERMO LT ARROW 90 M	THERMO RT ARROW 90 M	THERMO STR & RT ARROW 90 M	4" WHITE PAINT	4" YELLOW PAINT	8" YELLOW PAINT	24" WHITE PAINT	SNOW PLOWABLE MARKERS			
									MI	FT	SF	LS	LF	LF	LF	EA	EA	EA	LF	LF	LF	LF	EA			
2021CPT.09.03.10301	Davie	1	US601	FROM APPROX. 100' N. OF MCCULLOUGH RD SR1135 TO END OF LANE TAPER S. OF NC801	1, 2	2	2WU	2.15	27	264	1	27,284	28,651	100	150	4	2	6	27,284	28,651	100	150	212			
TOTAL FOR MAP NO. 1									2.15		264	1	27,284	28,651	100	150	4	2	6	27,284	28,651	100	150	212		
2021CPT.09.03.10301	Davie	2	US64	FROM APPROX. 200' N OF NC901 TO IREDELL COUNTY LINE	1	2	2WU	2.202	26	247		23,253	23,253						23,253	23,253			150			
TOTAL FOR MAP NO. 2									2.202		247		23,253	23,253						23,253	23,253			150		
TOTAL FOR PROJ NO. 2021CPT.09.03.10301									4.352		511	1	50,537	51,904	100	150	4	2	6	50,537	51,904	100	150	362		
											102,441					12			102,441							
2021CPT.09.04.20301	Davie	3	DEADMON RD SR 1801	FROM US601 TO START OF CURVE PRIOR TO FRANK SHORT RD SR1803	3	2	2WU	2.254	22	252		24,253	24,253													
TOTAL FOR MAP NO. 3									2.254		252		24,253	24,253												
2021CPT.09.04.20301	Davie	4	CANA RD SR1408	FROM US601 TO PVT JOINT AT BRIDGE # 50	4	2	2WU	3.358	22	1,259									35,460	35,460						
TOTAL FOR MAP NO. 4									3.358		1,259									35,460	35,460					
TOTAL FOR PROJ NO. 2021CPT.09.04.20301									5.612		1,511		24,253	24,253							35,460	35,460				
											48,506								70,920							
2021CPT.09.04.20302	Davie	4	CANA RD SR 1408	FROM US601 TO PVT JOINT AT BRIDGE #50	4	2	2WU	3.358	22																	
TOTAL FOR MAP NO. 4									3.358																	
TOTAL FOR PROJ NO. 2021CPT.09.04.20302									3.358																	
GRAND TOTAL									13.322		2,022	1	74,790	76,157	100	150	4	2	6	85,997	87,364	100	150	362		
											150,947					12			173,361							

Note: All quantities listed include turn lanes and are estimates; Payment will be based on actual field measurements and quantities received

SIGNING FOR RESURFACING PROJECTS

LEGEND
 T STATIONARY SIGN
 → DIRECTION OF TRAFFIC FLOW



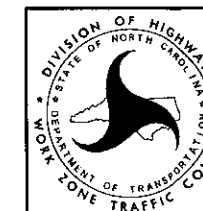
MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	 	<p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p> <p>#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p>	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> LESS THAN 1000' OF RESURFACING ALONG -Y- LINE SUBDIVISION ROADS DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> PLACED 500' IN ADVANCE OF FLAGGER. </div> <div style="text-align: center;"> PLACED 250' IN ADVANCE OF FLAGGER. </div> </div>
		<p>- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER.</p> <p>- AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.</p>	
		<p>- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS.</p> <p>- INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE.</p> <p>- FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH.</p> <p>- A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p> <p>- FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.</p>	
		<p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.</p>	
	<p>THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.</p>		

MAPS LESS THAN 2 MILES

FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.



ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING